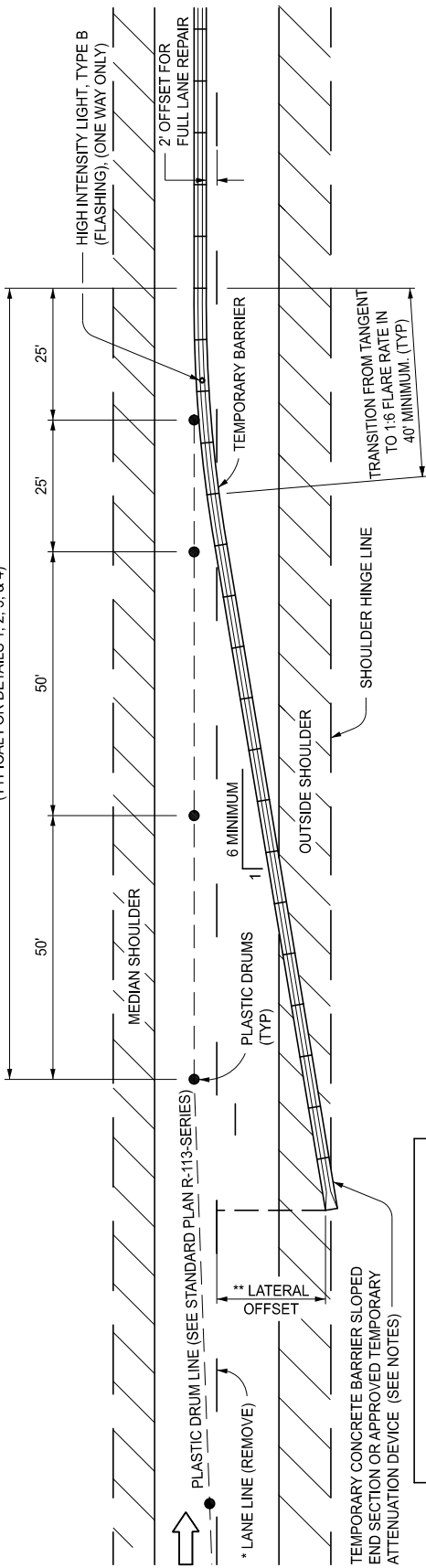


150' ON TANGENT
(TYPICAL FOR DETAILS 1, 2, 3, & 4)

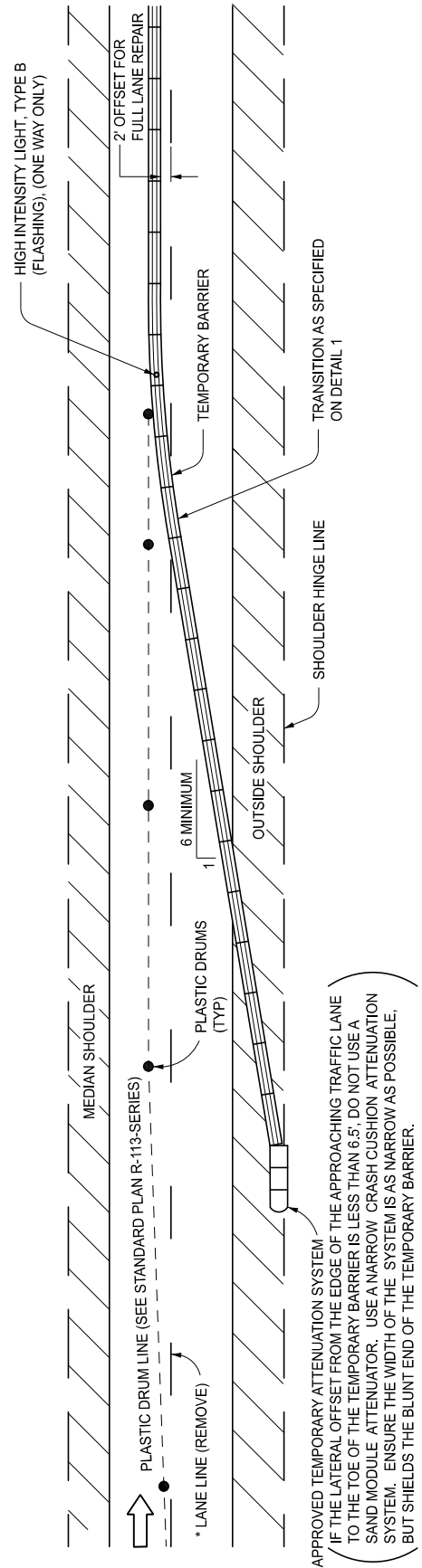


* LANE LINE SHALL BE REMOVED A MINIMUM LENGTH EQUAL TO THE LENGTH OF THE DRUM LINE TAPER; TYPICAL FOR DETAILS 1, 2, 3, 4, AND 5.

** THE LATERAL OFFSET REFERS TO THE MINIMUM DISTANCE BETWEEN THE EDGE OF THE NEAREST TRAVELED (OPEN) LANE AND THE TRAFFIC SIDE OF THE ENDING OF THE TEMPORARY BARRIER. THE BARRIER SHALL BE EXTENDED TO THE EDGE OF THE SHOULDER EVEN IF THIS DISTANCE IS BEYOND THE LATERAL OFFSET. IF THE LATERAL OFFSET DISTANCE IS GREATER THAN THE DISTANCE TO THE SHOULDER HINGE LINE, USE DETAIL 2.

DETAIL 1

** LATERAL OFFSET (MINIMUM DISTANCE)	
SPEED (MPH)	DISTANCE (FEET)
≤ 35	15
40	18
45	20
50	23
≥ 55	31



DETAIL 2

WHEN DETAIL 1 CANNOT BE USED BECAUSE OF RESTRICTED LATERAL OFFSET

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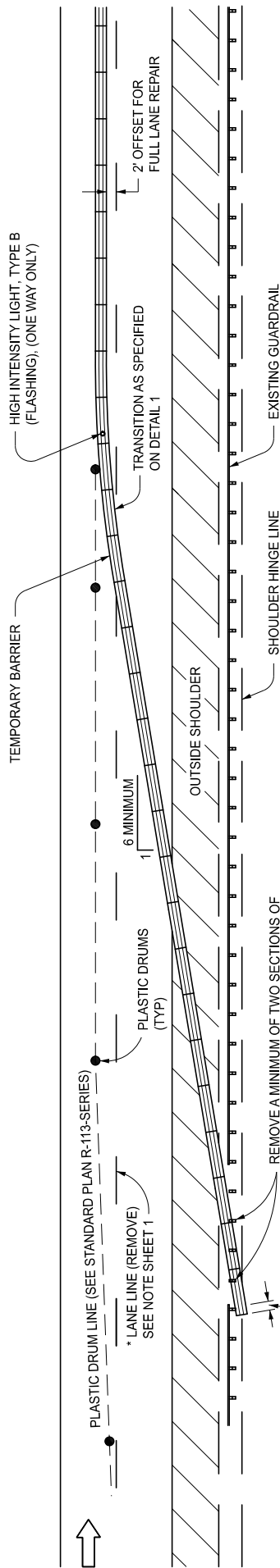


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DIRECTOR, BUREAU OF DEVELOPMENT

DEPARTMENT DIRECTOR
BRADLEY C. WIEFERICH, PE

STANDARD PLAN FOR
PLACEMENT OF TEMPORARY CONCRETE BARRIER
AND TEMPORARY STEEL BARRIER

(SPECIAL DETAIL)	08/25/2015	R-126-I	SHEET 1 OF 5
FHWA APPROVAL	PLAN DATE		

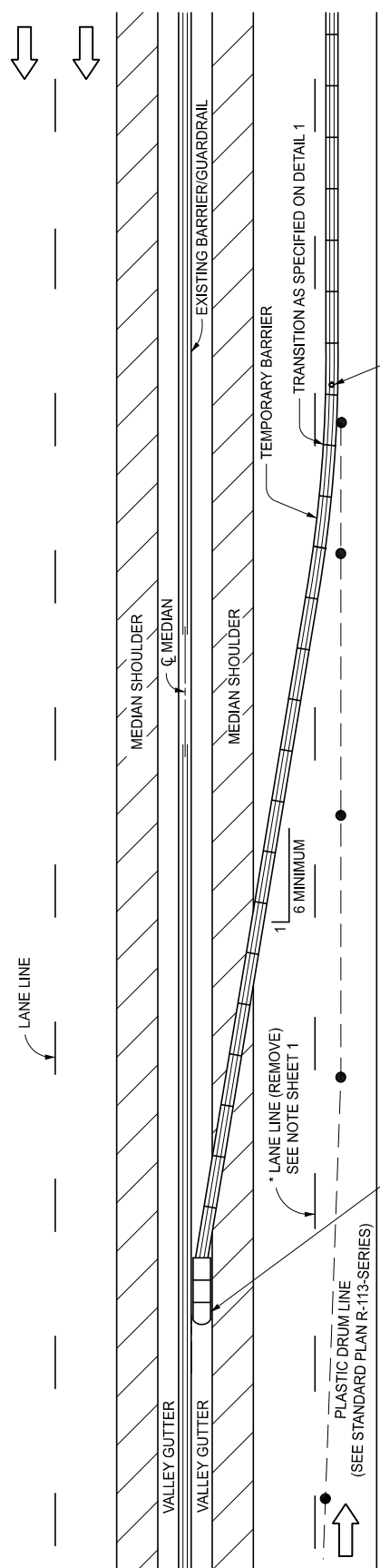


REMOVE A MINIMUM OF TWO SECTIONS OF GUARDRAIL AND POSTS IN THE RESULTING OPENING. (REPLACE THE GUARDRAIL WHEN TEMPORARY BARRIER IS REMOVED.)

THE TEMPORARY BARRIER SHALL EXTEND A MINIMUM OF 2' BEYOND THE GUARDRAIL POST. PLACE BARRIER AGAINST POST. DO NOT BREAK INTO THE LENGTH OF A GUARDRAIL ENDING TERMINAL. BEGIN BARRIER A MINIMUM OF ONE SECTION OF RAIL BEYOND THE LENGTH OF THE APPROACH TERMINAL OR REVISE THE TAPER RATE OF THE BARRIER.

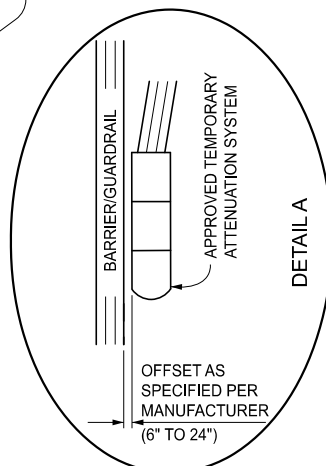
DETAIL 3

(TEMPORARY BARRIER IN GUARDRAIL AREA)



APPROVED TEMPORARY ATTENUATION SYSTEM (SEE DETAIL A)
 (IF THE LATERAL OFFSET FROM THE EDGE OF THE APPROACHING TRAFFIC LANE TO THE TOE OF THE EXISTING BARRIER/GUARDRAIL IS LESS THAN 8', DO NOT USE A SAND MODULE ATTENUATOR. USE A NARROW CRASH CUSHION ATTENUATION SYSTEM. ENSURE THE WIDTH OF THE SYSTEM IS AS NARROW AS POSSIBLE, BUT SHIELDS THE BLUNT END OF THE TEMPORARY BARRIER.)

NOTE: VALLEY GUTTER MAY BE ADJACENT TO PAVEMENT.



DETAIL 4

(TEMPORARY BARRIER IN MEDIAN BARRIER AREA)

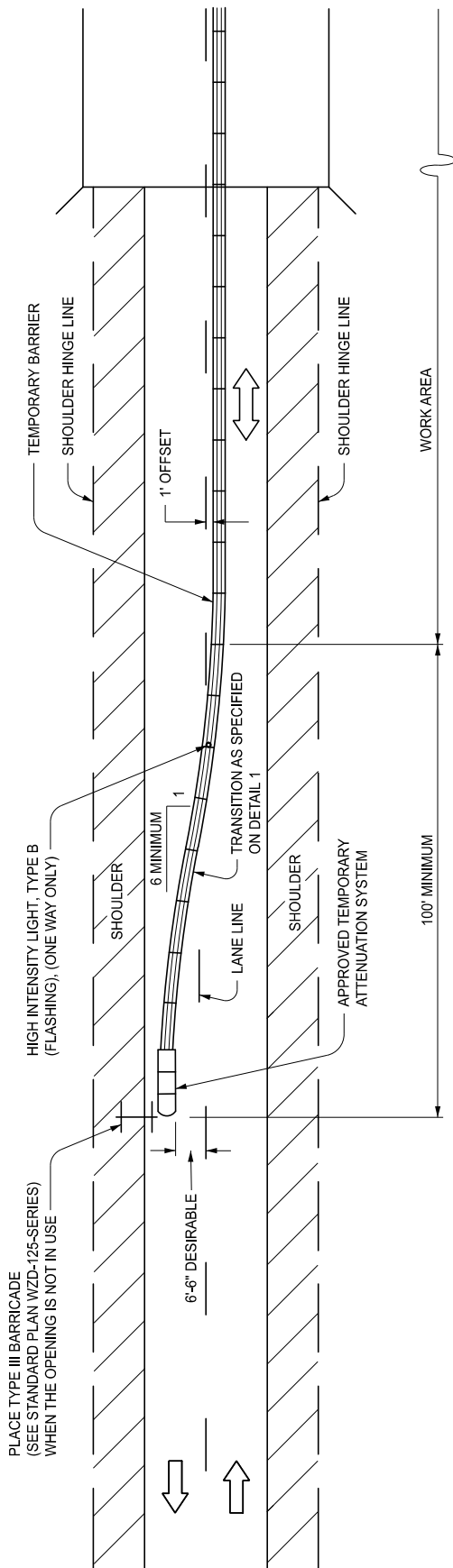
NOTE: SIMILAR TREATMENT MAY BE USED ON SINGLE FACE BARRIER ON OUTSIDE SHOULDER.



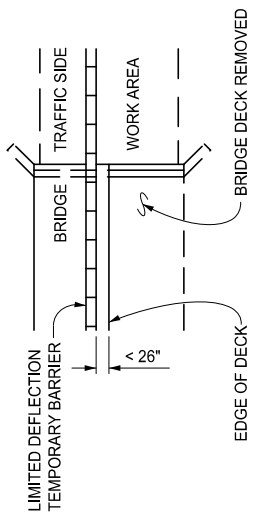
DEPARTMENT DIRECTOR
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STANDARD PLAN FOR
**PLACEMENT OF TEMPORARY CONCRETE BARRIER
AND TEMPORARY STEEL BARRIER**

(SPECIAL DETAIL)	08/25/2015	R-126-I	SHEET 2 OF 5
FHWA APPROVAL	PLAN DATE		



DETAIL 5
 (TRAILING END OF TEMPORARY BARRIER ON TWO-WAY ROADWAY)



TEMPORARY BARRIER ADJACENT TO A PRECIPITOUS DROP-OFF

DETAIL 5 NOTES:

WHERE CONTROLLED BIDIRECTIONAL TRAFFIC IS MAINTAINED ON A SINGLE LANE, THE PLACING OF TEMPORARY BARRIER ON THE APPROACHING END OF A LANE CLOSURE SHALL BE THE SAME AS SPECIFIED ON DETAIL 1, 2 OR 3. THE TRAILING END OF THE LANE CLOSURE SHALL BE AS SPECIFIED ON DETAIL 5. THE PLASTIC DRUM LINE TAPER WILL BE ACCORDING TO THE CURRENT MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES OR WITH THE MAINTAINING TRAFFIC PROVISIONS IN THE PLANS OR PROPOSAL. THE TEMPORARY BARRIER END SECTION ON THE TRAILING END OF THE TEMPORARY BARRIER SHALL BE PARALLEL WITH THE TRAFFIC.

BARRIER ADJACENT TO A PRECIPITOUS DROP-OFF:

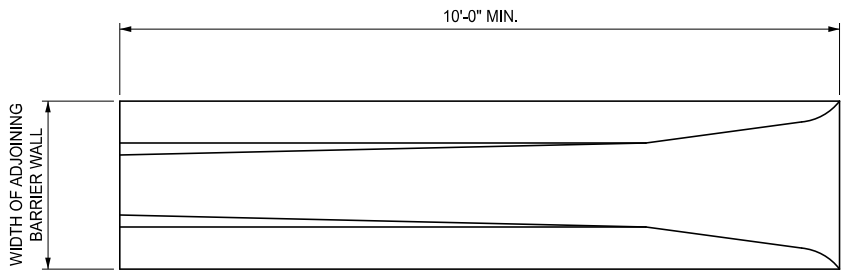
WHEN THERE IS 26" OR MORE, Laterally BETWEEN THE TOE OF THE BARRIER ON THE CONSTRUCTION SIDE AND THE DROP-OFF, STANDARD TEMPORARY CONCRETE BARRIER OR TEMPORARY STEEL BARRIER MEETING MDOT SPECIFICATIONS MAY BE USED NEAR THE DROP-OFF. NO SPECIAL HARDWARE OR PROCEDURES ARE NECESSARY. HOWEVER, WHEN THERE IS LESS THAN 26" Laterally BETWEEN THE TOE OF THE BARRIER ON THE CONSTRUCTION SIDE AND THE DROP-OFF, AN APPROPRIATE LIMITED DEFLECTION TEMPORARY BARRIER DETAIL MEETING THE REQUIREMENTS OF STANDARD PLAN R-53-SERIES, OR AN APPROVED ALTERNATIVE, MUST BE USED.

REFER TO STANDARD PLAN R-53-SERIES FOR ADDITIONAL INFORMATION REGARDING LIMITED DEFLECTION TEMPORARY BARRIER DETAILS.

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STANDARD PLAN FOR PLACEMENT OF TEMPORARY CONCRETE BARRIER AND TEMPORARY STEEL BARRIER		R-126-I	SHEET 3 OF 5
(SPECIAL DETAIL) FHWA APPROVAL	08/25/2015 PLAN DATE		



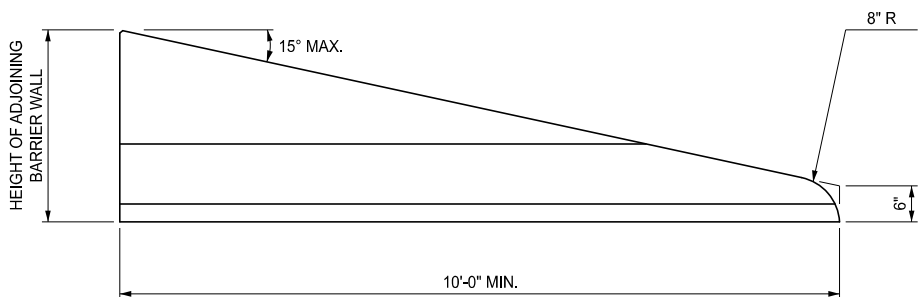
TOP VIEW

NOTES:

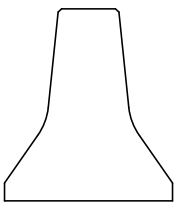
CONNECTIONS AND HARDWARE USED TO CONNECT THE SLOPED END SECTION TO TEMPORARY CONCRETE BARRIER (TCB) MUST MATCH CONNECTIONS BETWEEN TCB SEGMENTS AND MEET THE REQUIREMENTS OF NCHRP 350 OR MASH (TEST LEVEL 3 OR HIGHER).

STIRRUP DETAILS PROVIDED FOR ILLUSTRATION PURPOSES. ACTUAL STIRRUP DETAILS WILL VARY BASED ON END SECTION SHAPE, DIMENSIONS, AND STIRRUP LOCATION.

STIRRUPS MUST BE DESIGNED TO PROVIDE A MINIMUM OF 2 INCHES OF CLEAR COVER FROM ANY LOCATION ON THE SURFACE OF THE END SECTION.



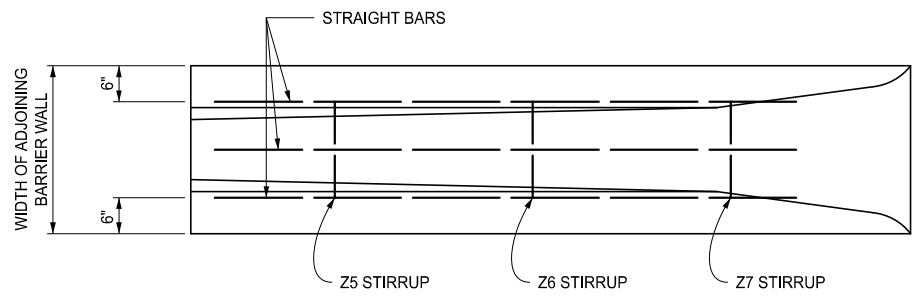
ELEVATION VIEW



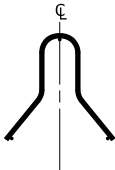
END VIEW

MUST MATCH CROSS-SECTIONAL SHAPE AND DIMENSIONS OF ADJOINING BARRIER WALL

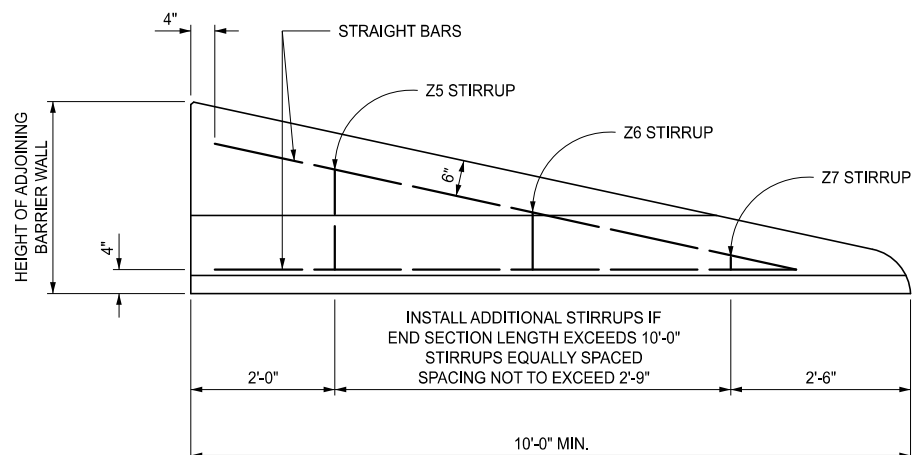
TEMPORARY CONCRETE BARRIER SLOPED END SECTION



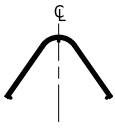
TOP VIEW



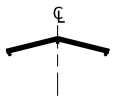
Z5 STIRRUP DETAIL



ELEVATION VIEW



Z6 STIRRUP DETAIL



Z7 STIRRUP DETAIL

STEEL REINFORCEMENT FOR TEMPORARY CONCRETE BARRIER SLOPED END SECTION

NOTE: #5 BARS SHALL BE USED FOR ALL STRAIGHT BARS AND STIRRUPS

<p>DEPARTMENT DIRECTOR BRADLEY C. WIEFERICH, PE</p>	STANDARD PLAN FOR PLACEMENT OF TEMPORARY CONCRETE BARRIER AND TEMPORARY STEEL BARRIER		R-126-I	SHEET 4 OF 5
	(SPECIAL DETAIL) FHWA APPROVAL	08/25/2015 PLAN DATE		

NOTES:

THE DETAILS IN STANDARD PLAN R-126-SERIES ARE NOT APPLICABLE TO PORTABLE WATER-FILLED BARRIER. PLACEMENT OF PORTABLE WATER-FILLED BARRIER SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

THE SEQUENCE OF BARRIER PLACEMENT SHALL BE IN THE DIRECTION OF TRAFFIC FLOW. BARRIER REMOVAL SHALL BE IN THE DIRECTION OPPOSITE TO TRAFFIC FLOW.

TEMPORARY BARRIER ENDINGS SUBJECT TO APPROACH TRAFFIC AND NOT TERMINATED BEHIND GUARDRAIL MUST BE ATTENUATED OR, IF APPLICABLE, SHIELDED WITH A TEMPORARY CONCRETE BARRIER SLOPED END SECTION.

DETAIL 2 WILL BE USED WHEN THERE IS NO EXISTING GUARDRAIL OR BARRIER AVAILABLE AS SPECIFIED IN DETAILS 3 OR 4.

THE LOCATION OF THE HIGH INTENSITY LIGHT, TYPE B (FLASHING) SPECIFIED IN DETAIL 1, SHALL APPLY TO DETAILS 2, 3, 4, AND 5. ALSO THE HIGH INTENSITY LIGHT, TYPE B SHALL BE FASTENED ON THE TOP OF THE BARRIER.

THE BARRIER FLARE RATE SHALL BE AT LEAST 6' OF RUN FOR EVERY FOOT OF OFFSET.

EXACT LATERAL PLACEMENT OF BARRIER MUST BE SPECIFIED ON THE PLANS OR IN THE PROPOSAL. BARRIER POSITION SPECIFIED IS DESIRABLE WHEN WORK AREA INVOLVES THE FULL WIDTH OF THE CLOSED LANE. WHEN PARTIAL LANE WORK IS INVOLVED, OR SHOULDER WORK ONLY, BARRIER SHOULD BE SET INSIDE CLOSED LANE, AS ALLOWED BY WORK AREA, WITH 2' OR MORE OFFSET DESIRABLE TO LANE LINE.

IN DETAIL 3, EXISTING GUARDRAIL POSTS ARE REMOVED TO FACILITATE PLACING OF TEMPORARY BARRIER BEHIND THE GUARDRAIL. THE DEGREE OF THE BARRIER DEPARTURE ANGLE WILL DETERMINE THE AMOUNT OF BEAM GUARDRAIL AND THE NUMBER OF POSTS TO BE REMOVED.

THE HIGH INTENSITY LIGHT, TYPE B SHALL BE OMITTED WHERE TWO-WAY TRAFFIC IS MAINTAINED ON ONE LANE AND IS CONTROLLED BY TRAFFIC SIGNALS.

IN DETAIL 1, WHEN USING STEEL BARRIER, ATTACH AN APPROVED TEMPORARY ATTENUATION SYSTEM TO TEMPORARY STEEL BARRIER. ATTACHING A TEMPORARY CONCRETE BARRIER SLOPED END SECTION TO TEMPORARY STEEL BARRIER WILL BE PROHIBITED. WHEN USING CONCRETE BARRIER, ATTACH TEMPORARY CONCRETE BARRIER SLOPED END SECTION OR AN APPROVED TEMPORARY ATTENUATION SYSTEM TO THE TEMPORARY CONCRETE BARRIER.



DEPARTMENT DIRECTOR
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STANDARD PLAN FOR
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AND TEMPORARY STEEL BARRIER

(SPECIAL DETAIL)
FHWA APPROVAL

08/25/2015
PLAN DATE

R-126-I

SHEET
5 OF 5