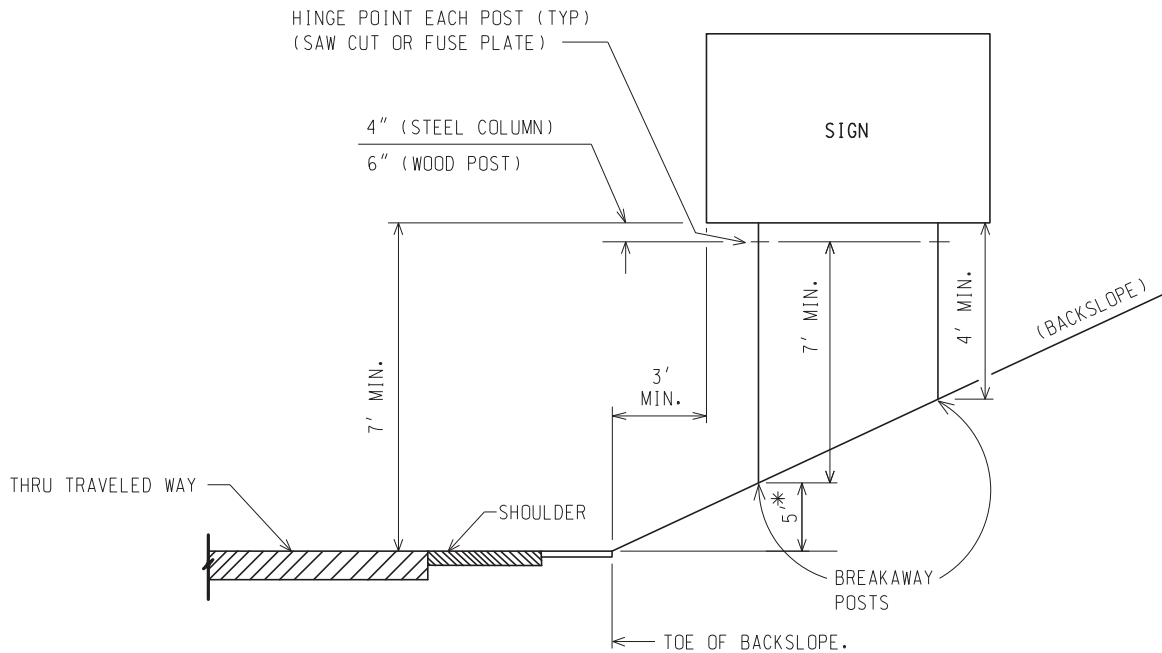


* THE LATERAL OFFSET SHALL BE 30' (MIN.), FOR MAINTENANCE PURPOSES.

**SIGN PLACEMENT-ELEVATION VIEW: FREEWAY
(FORESLOPE)**

<p>PREPARED BY DESIGN DIVISION</p> <p>DRAWN BY: <u>DHD</u></p> <p>CHECKED BY: <u>AJU</u></p>	<p>DEPARTMENT DIRECTOR Kirk T. Steudle</p> <p>APPROVED BY: <u>Kimberly Avery</u> DIRECTOR, BUREAU OF FIELD SERVICES</p> <p><small>Digitally signed by Kimberly Avery DN: ou=State of Michigan, email=averyk@michigan.gov, c=US, Kimberly Avery Date: 2018.05.09 12:11:33 -0400</small></p> <p>APPROVED BY: <u>Bradley C. Wiefelrich</u> DIRECTOR, BUREAU OF DEVELOPMENT</p>	<p>MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF DEVELOPMENT STANDARD PLAN FOR</p> <p>ROADSIDE SIGN LOCATIONS & SUPPORT SPACING</p>		
	<p>APPROVED BY: _____</p>	<p>05/24/18 F.H.W.A. APPROVAL</p>	<p>02/13/17 PLAN DATE</p>	<p>SIGN-120-E SHEET 1 OF 9</p>



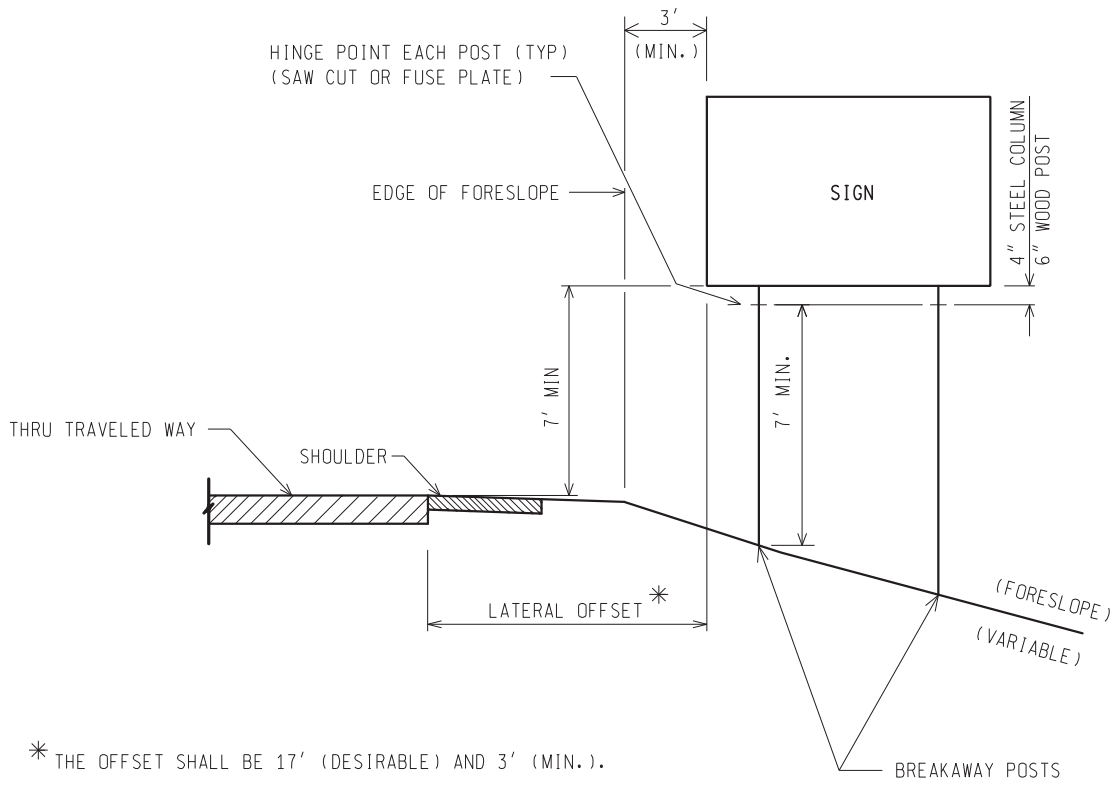
* IN DEPRESSED SECTIONS WITH 3:1 OR STEEPER BACKSLOPE, IF THE FRONT (TRAFFIC SIDE) POST CANNOT BE LOCATED AT 7' ABOVE SHOULDER ELEVATION, THEN A 3' MINIMUM OFFSET FROM THE TOE OF SLOPE SHALL BE MAINTAINED AND THE SIGN PROTECTED WITH AN APPROVED BARRIER. SEE GENERAL NOTE 5.

SIGN PLACEMENT-ELEVATION VIEW: FREEWAY (BACKSLOPE)

NOT TO SCALE

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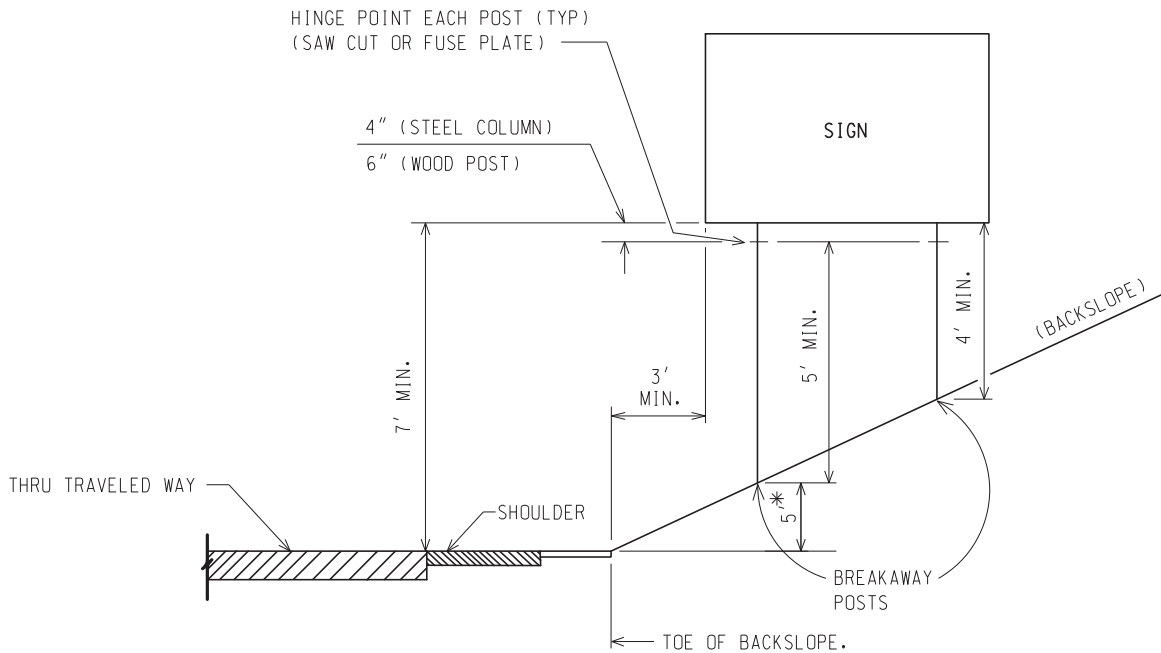


SIGN PLACEMENT-ELEVATION VIEW: NON-FREEWAY
(FORESLOPE)

NOT TO SCALE

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* IN DEPRESSED SECTIONS WITH 3:1 OR STEEPER BACKSLOPE, IF THE FRONT (TRAFFIC SIDE) POST CANNOT BE LOCATED AT 5' ABOVE SHOULDER ELEVATION, THEN A 3' MINIMUM OFFSET FROM THE TOE OF SLOPE SHALL BE MAINTAINED AND THE SIGN PROTECTED WITH AN APPROVED BARRIER. SEE GENERAL NOTE 5.

SIGN PLACEMENT-ELEVATION VIEW: NON-FREEWAY (BACKSLOPE)

NOT TO SCALE

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF DEVELOPMENT STANDARD PLAN

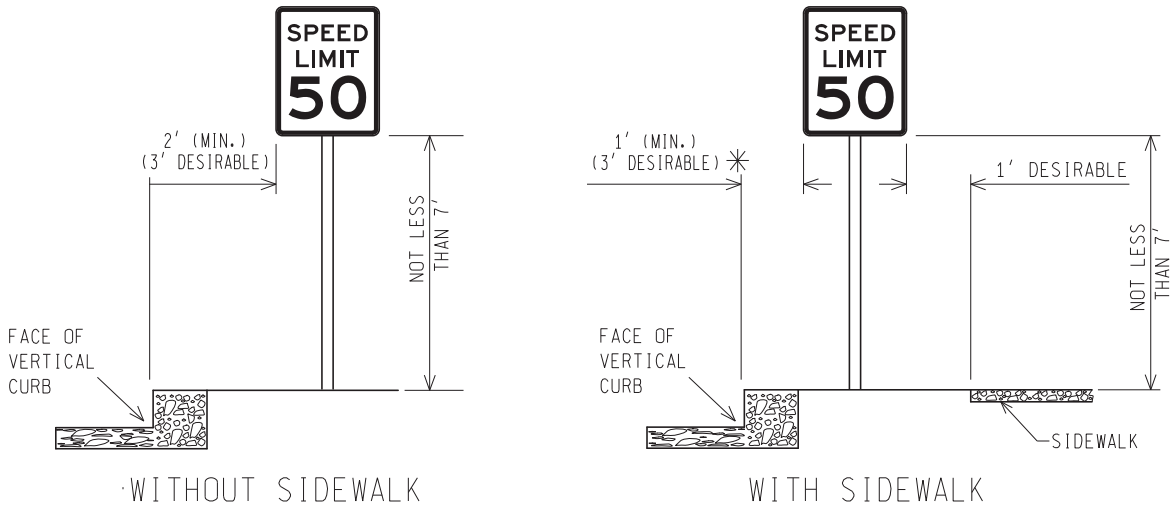
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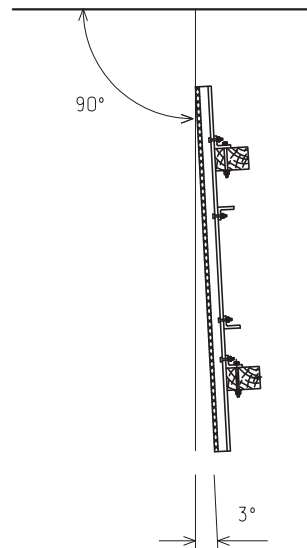
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*WHEN DIRECTED BY THE ENGINEER, 1' IS PERMITTED IN AREAS WHERE SIDEWALK OR UTILITY POLES ARE CLOSE TO CURB PER MMUTCD.

NOTE: SLOPING CURBS SHOULD BE TREATED AS FLAT.

SIGN PLACEMENT ALONG VERTICAL CURB ELEVATION VIEW



SIGN ORIENTATION

NOT TO SCALE

MICHIGAN DEPARTMENT OF TRANSPORTATION
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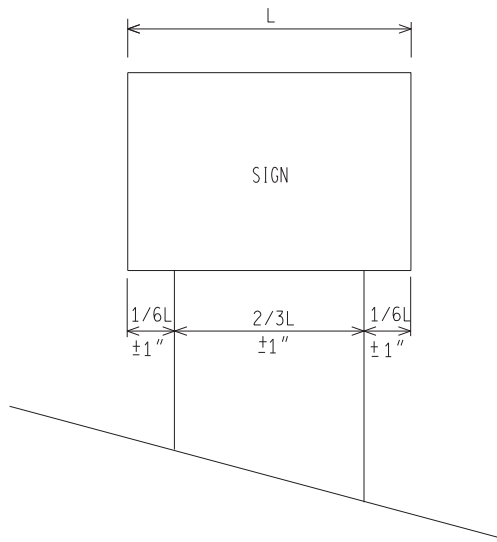
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2 POST SIGN SUPPORT SPACING

NOT TO SCALE

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SIGN BOTTOM HEIGHTS

CONVENTIONAL ROADS

7' - RURAL AREAS

7' - URBAN AREAS

7' - ALL CONDITIONS WHERE SIDEWALKS EXISTS

RAMPS/CROSSROADS

7' - RAMP AND CROSSROAD SIGNING
(WITHOUT VERTICAL CURB)

7' - RAMP AND CROSSROAD SIGNING
(WITH VERTICAL CURB)

7' - ALL CONDITIONS WHERE SIDEWALK EXISTS

4' - DO NOT ENTER AND WRONG WAY SIGNS
(FOR FREEWAY RAMPS)

FREEWAYS/EXPRESSWAYS

7' - ROUTE MARKERS, WARNING AND
REGULATORY SIGNS

7' - ALL OTHER FREEWAY/EXPRESSWAY SIGNS

NOTES:

1. PARKING SIGNS MOUNTED BELOW A PARENT SIGN MAY HAVE A BOTTOM HEIGHT 1' OR 1.5' (DEPENDING ON SIGN SIZE) LESS THAN BOTTOM HEIGHTS LISTED FOR PARENT SIGNS.
2. BOTTOM HEIGHT OF ALL SIGNS ARE 7' EXCEPT THE FOLLOWING:
OBJECT MARKERS- 4'
MILE POST MARKERS- 4'
WRONG WAY/DO NOT ENTER (FRWY RAMPS)- 4'
3. CONVENTIONAL ROAD-A STREET OR HIGHWAY OTHER THAN A FREEWAY OR EXPRESSWAY.
4. EXPRESSWAY-A DIVIDED HIGHWAY WITH PARTIAL CONTROL OF ACCESS.
5. FREEWAY-A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS.

NOT TO SCALE

MICHIGAN DEPARTMENT OF TRANSPORTATION
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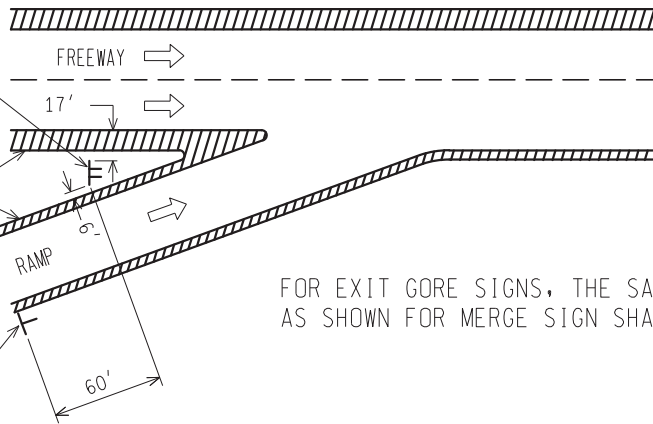


W4-1 AS CALLED FOR ON PLANS

EDGE OF PAVED SHOULDER(S)

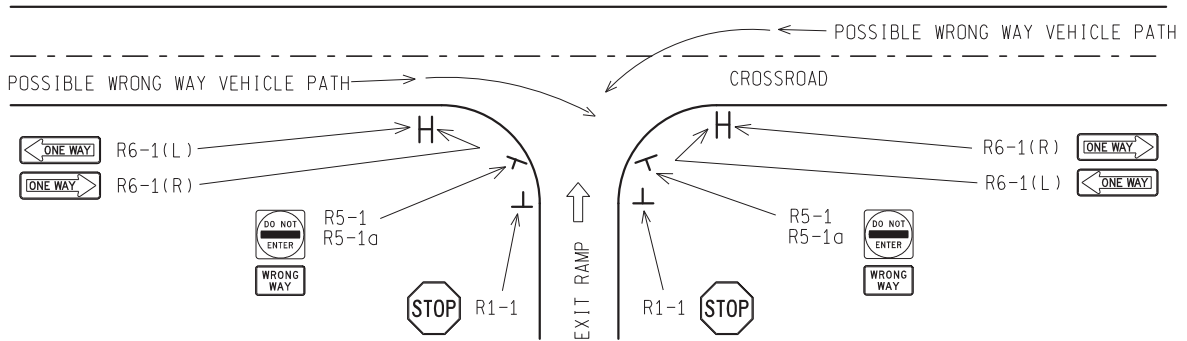


R3-2 (24") AS CALLED FOR ON PLANS. 15' OFFSET FROM EDGE OF RAMP PAVEMENT (NOT EDGE OF PAVED SHOULDER).



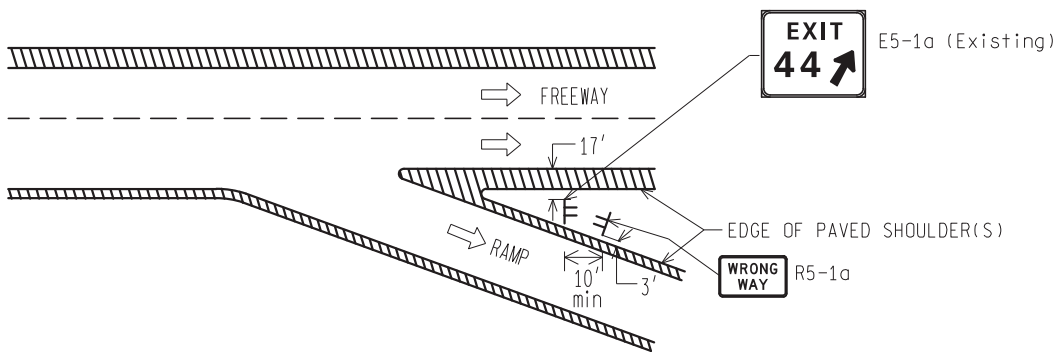
FOR EXIT GORE SIGNS, THE SAME OFFSETS AS SHOWN FOR MERGE SIGN SHALL BE USED

PLACEMENT OF MERGE & NO LEFT TURN SIGNS AT ENTRANCE RAMP



TYPICAL LOCATION OF R5-1 & R5-1a ON EXIT RAMP. THESE SIGNS SHOULD BE TURNED APPROXIMATELY 20 DEGREES FROM THE CROSSROAD TO FACE THE PATHS OF POSSIBLE WRONG WAY VEHICLE MOVEMENTS.

PLACEMENT OF SIGNS AT EXIT RAMP TERMINALS



PLACEMENT OF EXIT GORE & DO NOT ENTER SIGNS AT EXIT RAMP

NOT TO SCALE

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF DEVELOPMENT STANDARD PLAN

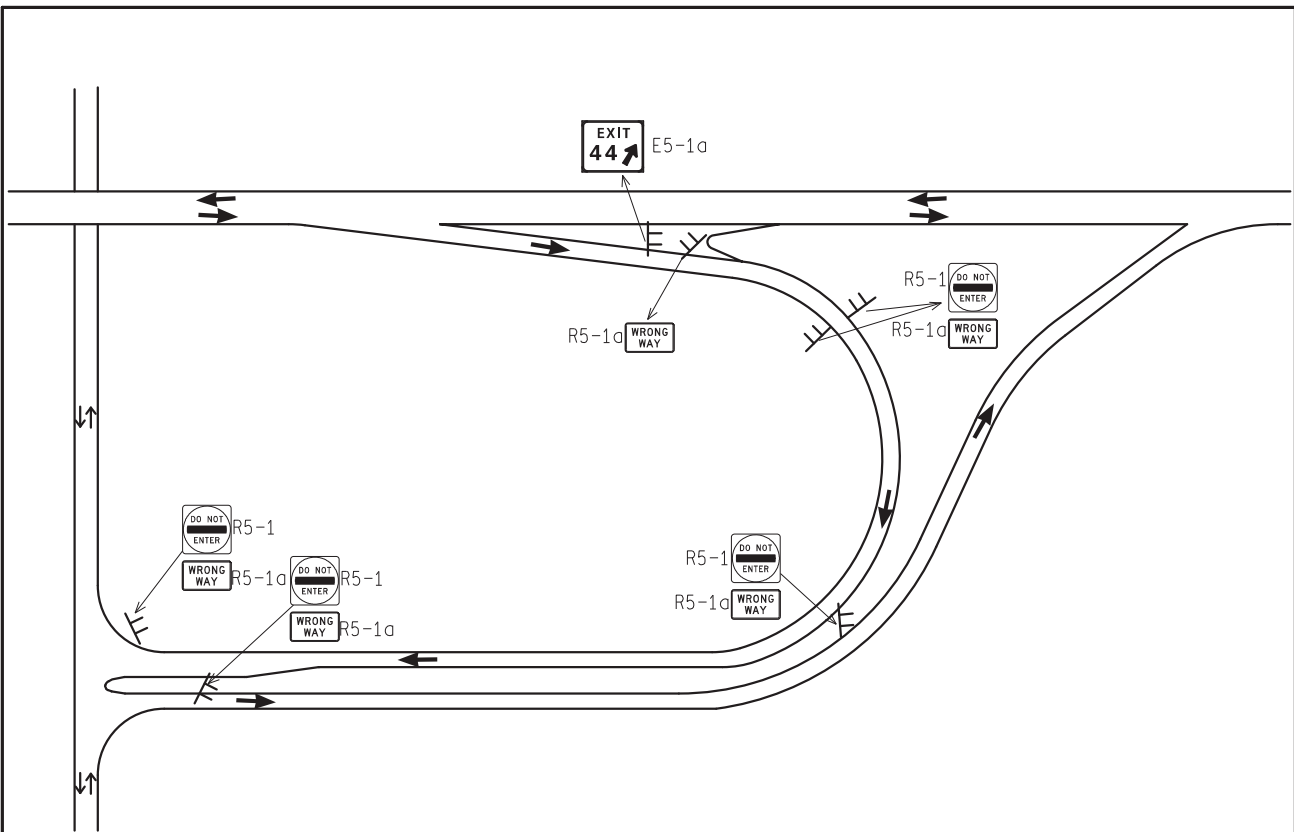
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PLACEMENT OF EXIT GORE, DO NOT ENTER & WRONG WAY SIGNS AT EXIT RAMP

GENERAL NOTES:

1. LATERAL OFFSET CLEARANCE OF ALL SIGNS SHALL BE AS INDICATED UNLESS OTHERWISE SHOWN ON CONTRACT SIGN PLAN SHEETS OR IN THE PROPOSAL.
2. THE TERM "SIGN" AS USED ON THIS PLAN MEANS A SINGLE PANEL OR GROUP OF PANELS COMBINED TO FORM ONE INSTALLATION.
3. BOTTOM HEIGHT (BH) SHALL BE AS INDICATED ON SHEET 7 UNLESS OTHERWISE SHOWN ON THE ELEVATION SIGN PLAN SHEET OR IN THE PROPOSAL. BOTTOM HEIGHT IS THE DIFFERENCE IN ELEVATION OF THE NEAREST EDGE OF THE TRAVELED LANE AND BOTTOM OF THE SIGN.
4. SIGN LOCATIONS SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON CONTRACT SIGN PLAN SHEETS OR IN THE PROPOSAL.
5. WHEN SIGNS ARE TO BE INSTALLED BEHIND CONCRETE BARRIER OR GUARDRAIL, THE NEAR EDGE OF SIGN SHOULD BE SET BACK A MINIMUM OF 3' MEASURED FROM THE BACK OF BARRIER OR GUARDRAIL POSTS. BREAKWAY SIGN POSTS ARE NOT REQUIRED AT THESE LOCATIONS.
6. FOR PLACEMENT OF STOP SIGNS AT CROSSROADS SEE MMUTCD.
7. WRONG WAY AND DO NOT ENTER SIGN SUPPORTS FOR FREEWAY RAMP SHALL HAVE RED REFLECTIVE STRIP(S) INSTALLED ON THE SIGN SUPPORTS.

NOT TO SCALE

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