

SE-SW
Sec. 1

SW-SE
Sec. 1

YPSILANTI TWP
T.3S.-R.7E.

NE-SE
Sec. 1

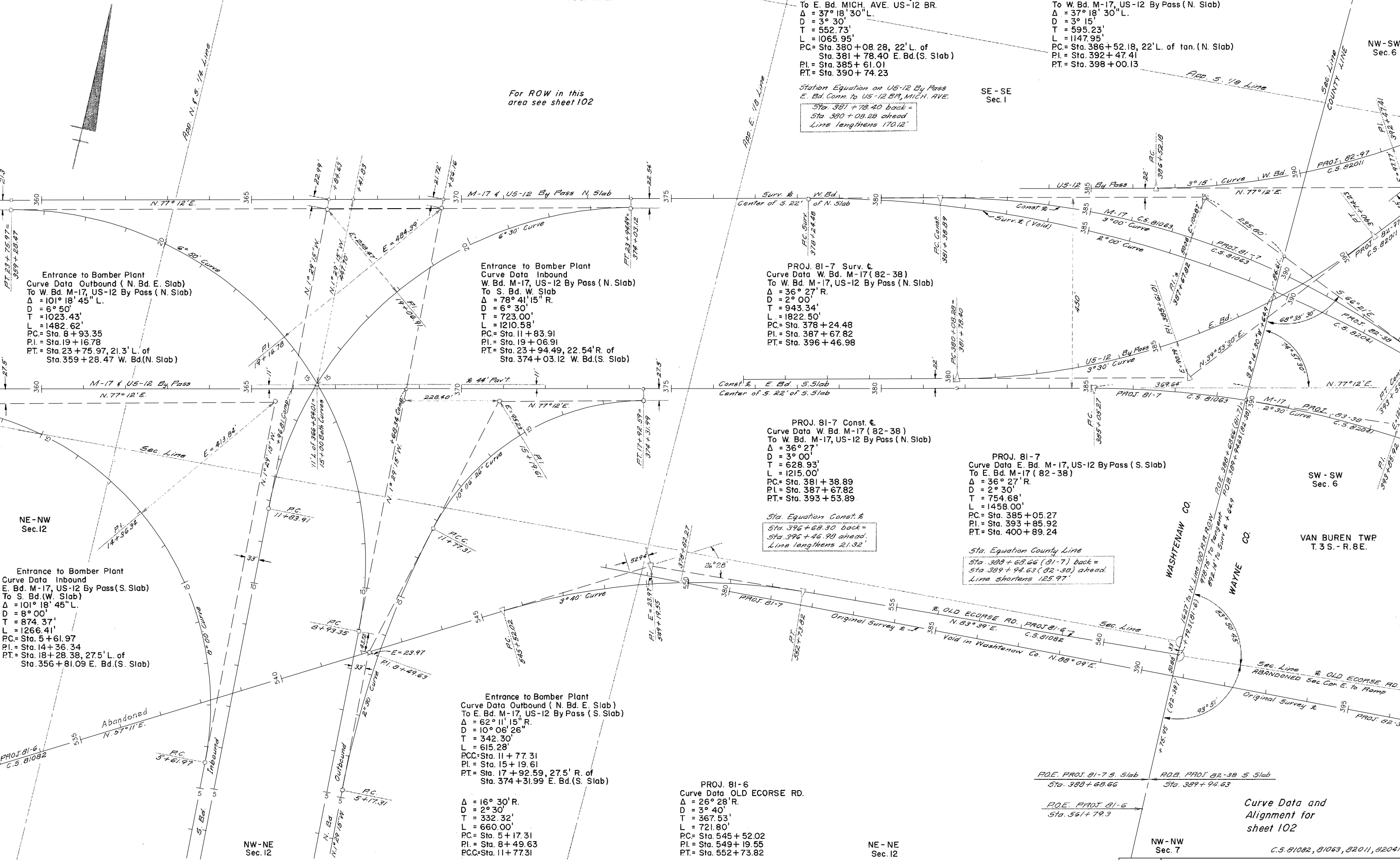
PROJ. 82-97
Curve Data MICH. AVE. US-12 BR. W. Bd.
To W. Bd. M-17, US-12 By Pass (N. Slab)
 $\Delta = 37^{\circ} 18' 30''$ L.
 $D = 3^{\circ} 15'$
 $T = 595.23'$
 $L = 1147.95'$
PC= Sta. 386+52.18, 22' L. of tan. (N. Slab)
PI= Sta. 392+47.41
PT= Sta. 398+00.13

NW-SW
Sec. 6

For ROW in this
area see sheet 102

Station Equation on US-12 By Pass
E. Bd. Conn. to US-12 BR, MICH. AVE.
Sta. 381+78.40 back =
Sta. 380+08.28 ahead
Line lengthens 170.12'

SE-SE
Sec. 1



Entrance to Bomber Plant
Curve Data Outbound (N. Bd. E. Slab)
To W. Bd. M-17, US-12 By Pass (N. Slab)
 $\Delta = 101^{\circ} 18' 45''$ L.
 $D = 6^{\circ} 50'$
 $T = 1023.43'$
 $L = 1482.62'$
PC= Sta. 8+93.35
PI= Sta. 19+16.78
PT= Sta. 23+75.97, 21.3' L. of
Sta. 359+28.47 W. Bd.(N. Slab)

Entrance to Bomber Plant
Curve Data Inbound
W. Bd. M-17, US-12 By Pass (N. Slab)
To S. Bd. W. Slab
 $\Delta = 78^{\circ} 41' 15''$ R.
 $D = 6^{\circ} 30'$
 $T = 723.00'$
 $L = 1210.58'$
PC= Sta. 11+83.91
PI= Sta. 19+06.91
PT= Sta. 23+94.49, 22.54' R. of
Sta. 374+03.12 W. Bd.(S. Slab)

PROJ. 81-7 Surv. &
Curve Data W. Bd. M-17 (82-38)
To W. Bd. M-17, US-12 By Pass (N. Slab)
 $\Delta = 36^{\circ} 27'$ R.
 $D = 2^{\circ} 00'$
 $T = 943.34'$
 $L = 1822.50'$
PC= Sta. 378+24.48
PI= Sta. 387+67.82
PT= Sta. 396+46.98

PROJ. 81-7 Const. &
Curve Data W. Bd. M-17 (82-38)
To W. Bd. M-17, US-12 By Pass (N. Slab)
 $\Delta = 36^{\circ} 27'$ R.
 $D = 3^{\circ} 00'$
 $T = 628.93'$
 $L = 1215.00'$
PC= Sta. 381+38.89
PI= Sta. 387+67.82
PT= Sta. 393+53.89

PROJ. 81-7
Curve Data E. Bd. M-17, US-12 By Pass (S. Slab)
To E. Bd. M-17 (82-38)
 $\Delta = 36^{\circ} 27'$ R.
 $D = 2^{\circ} 30'$
 $T = 754.68'$
 $L = 1458.00'$
PC= Sta. 385+05.27
PI= Sta. 393+85.92
PT= Sta. 400+89.24

Sta. Equation Const. &
Sta. 396+69.30 back =
Sta. 396+46.98 ahead.
Line lengthens 21.32'

Sta. Equation County Line
Sta. 388+68.66 (81-7) back =
Sta. 389+94.63 (82-38) ahead.
Line shortens 125.97'

Entrance to Bomber Plant
Curve Data Inbound
E. Bd. M-17, US-12 By Pass (S. Slab)
To S. Bd.(W. Slab)
 $\Delta = 101^{\circ} 18' 45''$ L.
 $D = 8^{\circ} 00'$
 $T = 874.37'$
 $L = 1266.41'$
PC= Sta. 5+61.97
PI= Sta. 14+36.34
PT= Sta. 18+28.38, 27.5' L. of
Sta. 356+81.09 E. Bd.(S. Slab)

Entrance to Bomber Plant
Curve Data Outbound (N. Bd. E. Slab)
To E. Bd. M-17, US-12 By Pass (S. Slab)
 $\Delta = 62^{\circ} 11' 15''$ R.
 $D = 10^{\circ} 06' 26''$
 $T = 342.30'$
 $L = 615.28'$
PCC= Sta. 11+77.31
PI= Sta. 15+19.61
PT= Sta. 17+92.59, 27.5' R. of
Sta. 374+31.99 E. Bd.(S. Slab)

PROJ. 81-6
Curve Data OLD ECORSE RD.
 $\Delta = 26^{\circ} 28'$ R.
 $D = 3^{\circ} 40'$
 $T = 367.53'$
 $L = 721.80'$
PC= Sta. 545+52.02
PI= Sta. 549+19.55
PT= Sta. 552+73.82

$\Delta = 16^{\circ} 30'$ R.
 $D = 2^{\circ} 30'$
 $T = 332.32'$
 $L = 660.00'$
PC= Sta. 5+17.31
PI= Sta. 8+49.63
PCC= Sta. 11+77.31

Curve Data and
Alignment for
sheet 102

C.S. 81082, 81063, 82011, 82041

RIGHT-OF-WAY MAP

DRAWN BY: WILCOX		CHECKED BY:		SHEET NO.	
LAST REVISION DATE: APRIL 2005	ROUTE: M-17	PROJ. JOB#: 81-6	CONTROL SECTION: 81082	102A	
COUNTY: WASHTENAW	US-12 BP	81-7	81063		
FILE NO.: 81-R-1					